

Contract No. 2435

**Replacement of Victoria Ave East Bridges over Pilot Butte Creek
and Victoria Ave Widening**

ADDENDUM # 1

May 13, 2015

This Addendum is issued prior to Tender closing, for the above captioned tender call, to provide for revisions and/or clarifications detailed herein.

All revisions and clarifications become part of the tender documents and all of their effect is to be included in the tender prices.

The receipt of this Addendum must be acknowledged on the Form of Tender when submitted.

1. CLOSING DATE

- a) The closing date has been extended to Tuesday, May 26, 2015 at 2:00:00 PM.

2. SECTION 00301 – SUPPLEMENTARY INSTRUCTIONS TO BIDDERS

- a) Replace Section 3 – Award Date with the following paragraph:

The award date is anticipated to be on or before June 5, 2015. Should the award date be later than June 5, 2015, an equivalent extension to the completion date will be provided.

3. SECTION 00400 – FORM OF TENDER

- a) Remove and replace the Form of Tender with the revised version attached to this addendum. Items that have changed from the original have been bolded.

4. SECTION 00700 – SPECIAL PROVISIONS

- a) Replace Section 3.0 – Adjustment of Completion Date with the following paragraph:

Bidders are advised that the intent is to have this Contract awarded no later than June 5, 2015. Any delay in award will be considered a delay beyond the Contractor's control as described in Clause 10.13, Delays, of the General Conditions.

- b) Insert the following two paragraphs following the first paragraph in Section 4.0 – Construction Staging

If the Separate Price is awarded, in order to maintain pedestrian and Transit traffic Eastgate Drive must remain open until the following conditions are met; the new pedestrian pathway across the north end of the culvert is operational, the new right out to westbound Victoria from southbound Eastgate is operational and the left turn on southbound Coleman Crescent to eastbound Victoria Avenue is restored.

In order to provide the required staging plan for the bridge replacements as shown on the Drawings, approximately 180 precast concrete F-type barriers are required (not including barriers marked as City Jersey Barrier). The Contractor is to supply 140 units (including tapered ends) as per the Tender Form; the City will supply the remaining 40 barriers. The Contractor will be responsible to pick up and delivery of all City supplied barriers at the beginning and end of the project from the City compound on Armour Road, as well as delivery to the City compound of barriers utilized during the detour but not left on site as permanent barriers. Any barriers damaged by the Contractor shall be replaced at the Contractors' cost.

- c) Insert the following paragraph after the second paragraph in Section 5.0 – Site Rental.

It is assumed that the Site Rental item contained in Schedule A, Table I accounts for all Work associated with the items contained in Table I, Table II and the Alternate price. As the Separate Price will only apply if the Alternate price does *not* apply, the Site Rental item contained in the Separate Price table will be additional days above those contained in Table I. No additional days will be added should the Separate Price not be accepted, and the Alternative Price applies.

- d) Replace Chain Link Fence Section 7.11.1 Scope with the following paragraph:

The Work will consist of supply of all equipment, labour, materials, and related works for the installation of approximately 32m total of 1.8m high Chain Link Fence as shown on the plans or as designated by the Engineer.

5. SECTION 00600 - GENERAL CONDITIONS

- a) Pursuant to the requirements of Clause 9.5.2, the general liability insurance shall be in the joint names of the Contractor, Owner, and Engineer (WSP Canada Inc.).

6. DRAWING D-0005 – DETAIL PLAN - WEST

- a) This drawing has been revised and re-issued as Rev. C. Notes were added regarding the curb and walk required for the Right In – Right Out median.

7. DRAWING D-0010 – STANDARD DETAILS & CROSS SECTION

- a) This drawing has been revised and re-issued as Rev. C. Additional details have been added to better clarify the type of curb required, and the fill required for the widening.

8. SECTION 00700 – PRE-TENDER MEETING

- a) A pre-tender meeting was held at City Hall on May 11, 2015. The meeting minutes have been attached to this addendum.

END OF ADDENDUM NO.1

MINUTES OF MEETING

**Replacement of Victoria Avenue East Bridges over Pilot Butte Creek
and Victoria Avenue Widening Pre-Tender Meeting
Schneider Board Room, City Call, 2476 Victoria Avenue, Regina, Saskatchewan
May 11, 2015 3:00 PM – 3:45 PM**

FILE NO:

PRESENT:

Jeremy Ellis – Gabriel Construction
Blaine Baumgartner – NIS Contractors Ltd.
Ron Smith – Pasqua Paving/WF Botkin
Samuel Tekle – WSP
Peter Leung – WSP
Blair Martin – WSP

Rodger Lindsey – Lindsey Construction
Brent Smith – Pasqua Paving
Tammy Moyse – City of Regina
Brad Walter – City of Regina
Brian Bolingbroke – WSP
Tyler Grant – GeoMetrix

Al Holzer – ASL Paving
Gerard Wappel – Wappel Construction
Brian Wappel – Wappel Construction
Brad Babcock – City of Regina
Parul Patel – WSP

DISTRIBUTION: As part of Addendum #1

Item Description

1. Introduction
2. Project Work
 1. Traffic Control. Stressed the requirement for staged construction, and maintaining two lanes of traffic open in each direction during construction
 2. Demolition. Staged demolition to adhere to traffic control requirements.
 3. Precast Concrete Box Culverts – As noted with above items, will need to construct from north to south to allow for detour lanes, and staged construction.
 4. Widening –Widening Victoria Avenue to gain eastbound third lane, which ends as right turn at Coleman Crescent, as well as westbound additional lane which ends as right turn lane at Prince of Wales Drive.
 5. Overlay – 50-70mm of milling and inlay, followed by a 150mm structural overlay.
 6. Right In/Right Out – Set up as an alternate price, consists of the removal of Eastgate Drive bridge, reclaim to match existing and constructing a right in and right out directly from Victoria Avenue to Eastgate Drive. Eastgate bridge is to be maintained for bulk of construction, to accommodate pedestrian access to Coleman Crescent.
 7. Pedestrian Access – pedestrian access across Victoria Avenue will be pushed to Prince of Wales Drive. Access to and from Coleman will be restricted during construction, will not require pedestrian access across Victoria during construction. As noted above, pedestrian access is required from Eastgate Drive to Coleman Crescent.
3. Specifications
 1. Construction Period – anticipated award of June 1, construction complete no later than October 30, 2015.
 2. Liquidated Damages – set at \$1000/day beginning October 31, 2015.
 3. Site Rental - Site rental will be charged to the Contractor for each calendar day during construction that traffic is restricted on Victoria Avenue resulting

from the Work. A restriction is an event which disrupts normal traffic flow on Victoria Avenue for any period of time. Restrictions include, but not limited to; detours, flagpersons, lane closures or width restrictions. Site rental will be at a rate of \$3,000.00 plus GST per calendar day.

4. Form of Tender – as noted earlier, there is both an alternate and a separate price that is required.
5. Separate Prices – for the Right In/Right Out, which is anticipated to go ahead. Should it be deleted, then the Alternate price will apply.
6. 0201 Asphalt Mix Design – use City of Regina standard mix design, 16mm asphalt for Arterial roadways.

4. Contractor Questions and Clarifications

1. Should there be a separate Site Rental item for the separate price?
 - We will look into it. Will be answered in addendum 1.
2. What type of gravel is required? Drawing shows gravel not as Regina spec.
 - Drawing will be corrected. Should be Type 33 granular base.
3. What is the structure for the embankment of the widening? The drawing shows note for topsoil and seeding only.
 - Note missing from drawing. Should read topsoil and hydroseed on clay fill embankment. Will be corrected in addendum 1.
4. How is the detour paid for?
 - Additional work required for the detour will be paid under the Traffic Accommodation bid item.
5. What is 4.2m walk on south side?
 - Just the width required to tie in to the existing 4.2m walk on Coleman Crescent.
6. What is 300mm curb? Detail shows 200mm curb?
 - Will be clarified in Addendum 1
7. What is 1.0m walk as shown on tender form? There is 1.2 and 1.5m walk on drawings only.
 - Mistake in tender form. Will be corrected in Addendum 1.
8. Will there be signage posted outside of project limits to ensure oversize equipment does not try and get through site?
 - Oversize trucks should be getting permit, at which time they will be required to take a different route. The City will consider posting signs as a courtesy, for those that may not get the proper permits.
9. Will the working hours be restricted to the City of Regina bylaw hours?
 - As the area is not residential, the City will consider allowing extended work hours upon request of the contractor.
10. Is the successful contractor considered Prime Contractor as per OH&S Regulations?
 - Yes.
11. Is the granular material listed in the Special Provisions supposed to be Sub-Drainage sand, or is it subbase?
 - It is supposed to be Sub Drainage Sand, as per Specification 2210.
12. Is WSP to be noted in the bond as Owners Consultant?
 - Yes. WSP is the Owners Representative, and this will be clarified in Addendum 1

5. Tender Submission Requirements

1. Tender Close. Was noted in meeting as being May 20, 2015 at 2:00:00 PM. Due to the long weekend upcoming, this has been amended to Thursday, May 21, 2015 at 2:00:00 PM.
2. Bid Bond. A bid bond of 10% is required as per the contract documents.
3. Consent of Surety for Performance & Labour and Materials Bond. As per the specification, this is required as part of the tender submission.

The above is considered to be a true and accurate record of discussions at the meeting. Please advise the writer of any discrepancies noted.

Signature Removed

Blair Martin, P.Eng.

BRM

**Replacement of Victoria Ave East Bridges over Pilot Butte Creek and Victoria Ave Widening
Contract No. 2435**

The Bidder, having carefully read and examined the Contract Documents and having visited and examined the Place of Work for Replacement of Victoria Ave East Bridges over Pilot Butte Creek and Victoria Ave Widening Contract No: 2435, offers all labour and Product required to complete the Work in accordance with the Contract Documents for the sums set out in the Form of Tender. The undersigned agrees on acceptance of its tender to execute the Contract and bonds in the form approved by the Owner.

Subject to the provisions of the Contract for the extension of time to complete, the Bidder agrees to complete all Work on or before **October 30, 2015.**

Bidder Information (Please Print)

| | |
|---------------|----------|
| Company Name: | |
| Address: | Contact: |
| | Phone: |
| | Fax: |
| | Email: |

The undersigned agrees to execute the Contract within 14 calendar days after the date of issue of the Notice of Award of Contract. The undersigned submits a Consent of Surety to furnish a Performance Bond and a Labour and Materials Payment Bond each in the sum of 50% of the Total Tender Price. The bonds will be delivered by the Bidder at the time of execution of the Contract. The Consent of Surety to be issued by the

_____ Surety Company, provided the company is satisfactory to the Owner.

As a bid security the following is submitted:

A certified cheque drawn on the _____ Bank, payable to the City of Regina, in the sum of \$ _____, being 10% of the Total Tender Price; or

A bid bond acceptable to the City of Regina from the _____ Bonding Company in the value of 10% of the Total Tender Price.

The following Addenda have been received. The modifications to the Form of Tender noted therein have been considered and the effects are included in the Total Tender Price.

Addendum Number _____ Dated _____

Addendum Number _____ Dated _____

Addendum Number _____ Dated _____

Addendum Number _____ Dated _____

This tender is irrevocable, made for good consideration and acceptance thereof by the City of Regina, and shall be binding on the undersigned from the date of acceptance.

SIGNED AND DELIVERED

this _____ day of _____ A.D., 20 _____.

Signature of Contractor

Name and Title (Please Print)

OR, In the case of an individual or individuals:

Signature of Contractor

Signature of Witness

Name and Title (Please Print)

Name of Witness (Please Print)

Signature of Contractor

Signature of Witness

Name and Title (Please Print)

Name of Witness (Please Print)

SCHEDULE A – SCHEDULE OF QUANTITIES AND PRICES - *REVISED*

Table I – Bridge Replacements

| Item No. | Spec. | Description | Unit | Qty. | Unit Price | Total Price |
|----------|-----------------------|--|----------------|------------|------------|-------------|
| 1 | 01025 | General Requirements | LS | 1 | \$ - | \$ |
| 2 | 01025 | Traffic Accommodation | LS | 1 | \$ - | \$ |
| 3 | 00700 | Site Rental | day | | \$ 3000 | \$ |
| 4 | 2110/ 2315 | Earth Excavation | m ³ | 1,080 | \$ | \$ |
| 5 | 00700 | Cofferdam + Water Management | LS | 1 | \$ - | \$ |
| 6 | 2230/ 00700 | Gravel Foundation | t | 1,800 | \$ | \$ |
| 7 | 00700 | 2.4m x 3.0m x 2.0m Box Culvert | No. | 131 | \$ | \$ |
| 8 | 00700 | 2.4m x 3.0m x 1.0m Box Culvert | No. | 2 | \$ | \$ |
| 9 | 00700 | 2.4m x 3.0m x 2.0m Box Culvert Beveled End | No. | 16 | \$ | \$ |
| 10 | 2220 | Granular Sub-base | t | 1,160 | \$ | \$ |
| 11 | 2130 | Sub-grade Preparation (150mm depth) | m ² | 2,080 | \$ | \$ |
| 12 | 2210 | Sub-Drainage Sand | t | 4,400 | \$ | \$ |
| 13 | 00700 | Backfill – Clay Seals | m ³ | 1,190 | \$ | \$ |
| 14 | 00700 | Removal of Bridge Structures | LS | 1 | \$ - | \$ |
| 15 | 00700 | Heavy Rock Riprap | m ³ | 102 | \$ | \$ |
| 16 | 2230/ 00700 | Granular Base Course | t | 510 | \$ | \$ |
| 17 | 2350/ 2325 | Supply and Placement of Asphaltic Concrete | t | 620 | \$ | \$ |
| 18 | 01025 | Remove and Dispose of Existing Guardrail | Lm | 200 | \$ | \$ |
| 19 | 00700 | Precast F-Type Barriers | No. | 140 | \$ | \$ |
| 20 | 00700 | Precast Lock-Block Retaining Walls | LS | 1 | \$ - | \$ |
| 21 | 00700 | Chain Link Fence | LS | 1 | \$ - | \$ |
| 22 | 2550 | Pedestrian Ramp | No. | 1 | \$ | \$ |
| 23 | 2300 | Prime/Tack Coat | m ² | 4,200 | \$ | \$ |
| 24 | 2070 | Removal of Concrete Curb and Gutter | Lm | 260 | \$ | \$ |
| 25 | 2550 | Concrete Curb and Gutter | Lm | 90 | \$ | \$ |
| 26 | 2070 | Remove Concrete Median | m ² | 70 | \$ | \$ |

| | | | | | | |
|-------------------------------|------|-----------------|----------------|-----|----|----|
| 27 | 2600 | Concrete Median | m ² | 278 | \$ | \$ |
| SUB-TOTAL - Items 1-27 | | | | | | \$ |

Table II - Victoria Avenue Widening & Overlay

| Item No. | Spec. | Description | Unit | Qty. | Unit Price | Total Price |
|--------------------------------|-----------------|---|----------------|--------|------------|-------------|
| 28 | 01025 | General Requirements | LS | 1 | \$ - | \$ |
| 29 | 01025 | Traffic Accommodation | LS | 1 | \$ - | \$ |
| 30 | 2120 | Embankment | m ³ | 540 | \$ | \$ |
| 31 | 2220 | Granular Sub-base | t | 3,450 | \$ | \$ |
| 32 | 2130 | Sub-grade Preparation (150mm depth) | m ² | 3,600 | \$ | \$ |
| 33 | 2230/ 00700 | Granular Base Course | t | 820 | \$ | \$ |
| 34 | 2110 | Earth Excavation | m ³ | 650 | \$ | \$ |
| 35 | 2350/ 2325 | Supply and Placement of Asphaltic Concrete | t | 7,000 | \$ | \$ |
| 36 | 02581 | Pavement Markings | LS | 1 | \$ - | \$ |
| 37 | 2300 | Prime/Tack Coat | m ² | 27,200 | \$ | \$ |
| 38 | 2075 | Cold Planing | m ² | 12,050 | \$ | \$ |
| 39 | 02934/ 02212 | Topsoil and Hydraulic Seed Mulch | m ² | 1,320 | \$ | \$ |
| 40 | 2650 | 600mm CSP Culvert | Lm | 26 | \$ | \$ |
| SUB-TOTAL - Items 28-40 | | | | | | \$ |

SUB-TOTAL – Items 1-40 \$ _____

5% GST \$ _____

TOTAL TENDER PRICE \$ _____

Abbreviations Used:

No. = number
m³ = cubic metre
m² = square metre

Lm = linear metre
t = tonne
LS = lump sum

SEPARATE PRICES:

The following are the separate prices for the work listed hereunder. Such work and amounts are not included in our Base Bid Price. Separate Price #1 is for the removal of Eastgate Drive Bridge and Eastgate Drive entering Coleman Crescent as well as the construction of Right In and Right Out lanes from Eastgate Drive to Victoria Avenue westbound as shown on the contract drawings. These prices include PST and exclude GST.

SEPARATE PRICE #1 – Eastgate Drive Right In/Right Out

| Item No. | Spec. | Description | Unit | Qty. | Unit Price | Total Price |
|-----------|-----------------|---|----------------------|------------|------------|-------------|
| 1 | 01025 | General Requirements | LS | 1 | \$ - | \$ |
| 2 | 01025 | Traffic Accommodation | LS | 1 | \$ - | \$ |
| 3 | 00700 | Removal of Eastgate Bridge Structure | LS | 1 | \$ - | \$ |
| 4 | 2110/ 02315 | Earth Excavation | m ³ | 200 | \$ | \$ |
| 5 | 2550 | Pedestrian Ramp | No. | 4 | \$ | \$ |
| 6 | 2070 | Removal of Existing Asphalt | m ³ | 210 | \$ | \$ |
| 7 | 2110 | Removal of Existing Base | m ³ | 320 | \$ | \$ |
| 8 | 2230/ 00700 | Granular Base Course | t | 460 | \$ | \$ |
| 9 | 2220 | Granular Sub-base | t | 1,900 | \$ | \$ |
| 10 | 2130 | Sub-grade Preparation (150mm depth) | m ² | 1,950 | \$ | \$ |
| 11 | 2350/ 2325 | Supply and Placement of Asphaltic Concrete | t | 720 | \$ | \$ |
| 12 | 02581 | Pavement Markings | LS | 1 | \$ - | \$ |
| 13 | 02934/ 02212 | Topsoil and Hydraulic Seed Mulch | m ² | 2,000 | \$ | \$ |
| 14 | 2075 | Cold Planing | m ² | 360 | \$ | \$ |
| 15 | 2550 | 2.0 m Monowalk | m² | 110 | \$ | \$ |
| 16 | 2550 | 1.2 m Monowalk | m² | 90 | \$ | \$ |
| 17 | 2550 | Concrete Curb and Gutter | Lm | 210 | \$ | \$ |
| 18 | 2550 | Combined Concrete 1.5m Walk and Curb | m² | 56 | | |
| 19 | 2070 | Remove Existing Curb and Gutter | Lm | 620 | \$ | \$ |
| 20 | 2300 | Prime/Tack Coat | m ² | 5,600 | \$ | \$ |
| 21 | 2600 | Concrete Median Curb | Lm | 60 | \$ | \$ |
| 22 | 2650 | 600 mm CSP Culvert | Lm | 51 | \$ | \$ |

| | | | | | | |
|----|-------|-------------------------------|-----|--|---------|----|
| 23 | 00700 | Site Rental (additional days) | day | | \$ 3000 | \$ |
|----|-------|-------------------------------|-----|--|---------|----|

SUB-TOTAL - Items 1-23 \$

5% GST \$

TOTAL Items 1-23 \$

ALTERNATE PRICES:

The following are the alternate prices for the work listed hereunder. Such work and amounts revise the Base Bid Price. These prices include PST and exclude GST. This alternate price is for the right turn lane on westbound Victoria Avenue at Coleman Crescent, and shall apply if Separate Price #1 is not accepted.

ALTERNATE PRICE #1 – Right Turn Lane Victoria Avenue Westbound

| Item No. | Spec. | Description | Unit | Qty. | Unit Price | Total Price |
|----------|----------------|--|----------------|-------|------------|-------------|
| 1 | 01025 | General Requirements | LS | 1 | \$ - | \$ |
| 2 | 01025 | Traffic Accommodation | LS | 1 | \$ - | \$ |
| 3 | 2230/ 00700 | Granular Base Course | t | 170 | \$ | \$ |
| 4 | 2220 | Granular Sub-base | t | 700 | \$ | \$ |
| 5 | 2130 | Sub-grade Preparation (150mm depth) | m ² | 720 | \$ | \$ |
| 6 | 2350/ 2325 | Supply and Placement of Asphaltic Concrete | t | 260 | \$ | \$ |
| 7 | 2300 | Prime/Tack Coat | m ² | 1,430 | \$ | \$ |

SUB-TOTAL - Items 1-7 \$

5% GST \$

TOTAL Items 1-7 \$

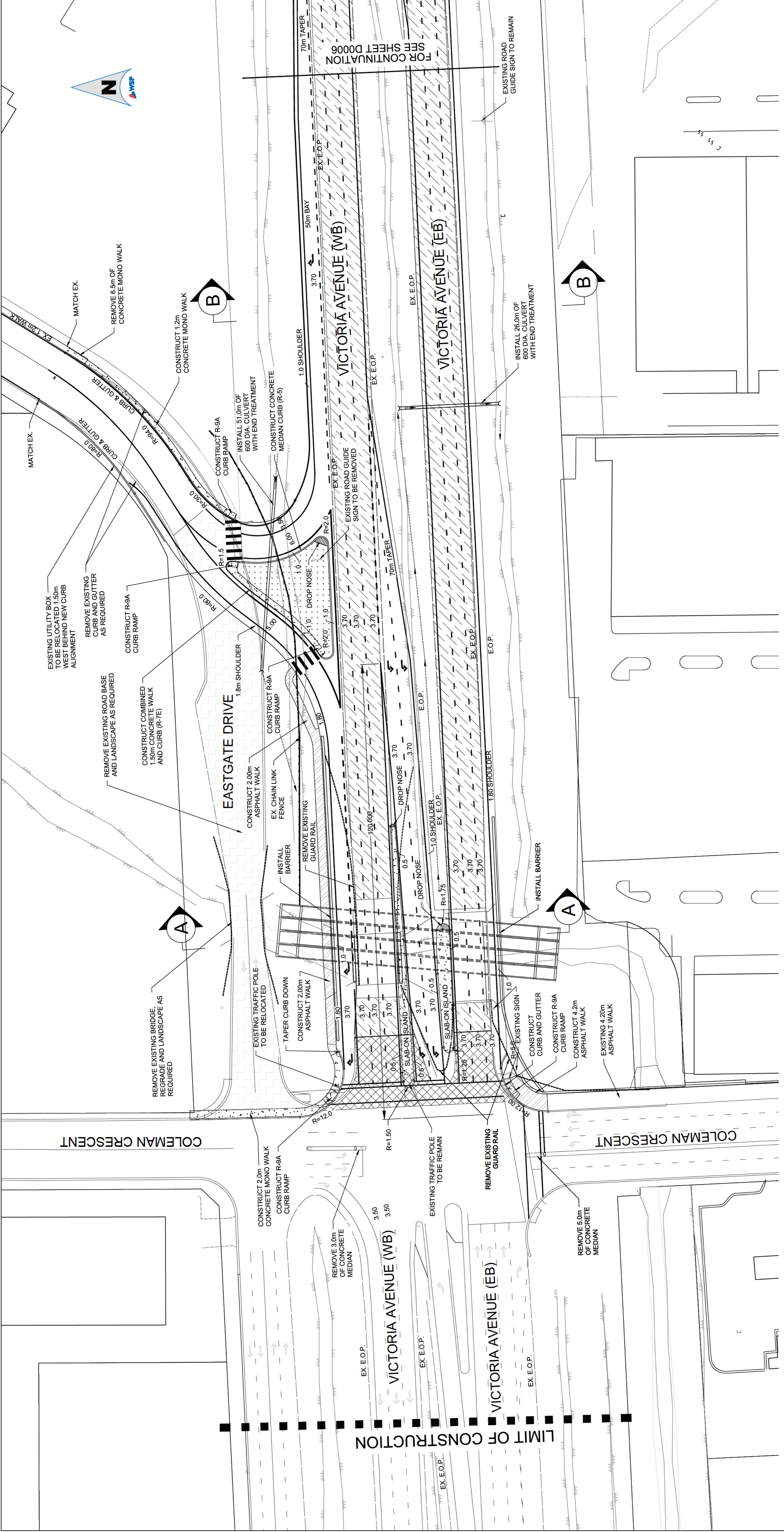
Effect on Base Bid

| | | |
|--------------------|-----------------|------------------|
| DESCRIPTION | ADDITION | DEDUCTION |
|--------------------|-----------------|------------------|

Right Turn Lane Victoria Avenue Westbound

\$ _____ \$ _____

(PST included, GST extra)



DETAIL PLAN

| LEGEND | |
|--------|--|
| | PERMANENT PAVEMENT STRUCTURE 150mm ACC 3-20A GRAVEL (VARIES) |
| | MILL 50mm - 70mm ASPHALT CONCRETE 50mm - 150mm |
| | MILL 50mm - 70mm INLAY 50mm - 70mm ASPHALT CONCRETE 150mm |
| | ASPHALT WALK |
| | CONCRETE |
| | LANDSCAPE |
| | EXISTING ROAD REMOVAL |

Notice:
 -Utilities may not be as shown.
 -City/Crews must coordinate utility locations for ALL utilities (including traffic control facilities) through the City Dispatch Office.
 -Contractors must coordinate utility locations through their own utility locators.
 -The City of Regina is NOT responsible for any damage related to any inaccuracies in the drawings or any third-party contractor's failure to check drawings in the field.
 -Drawings are in metric unless otherwise noted.
 -Elevations are metric geoidic.

Professional Seal
 Association of Professional Engineers & Geoscientists of Saskatchewan
CERTIFICATE OF AUTHORIZATION
 WSP Canada Inc.
 Number C0868
 Permission to Consult held by:
 Discipline Sk. Reg. No. Signature
 CIVIL 22577

| M/D/Y | Ref | By | For |
|----------|-----|------|------|
| 04/28/15 | A | V.X. | B.M. |
| 05/13/15 | A | V.X. | B.M. |

REVISIONS

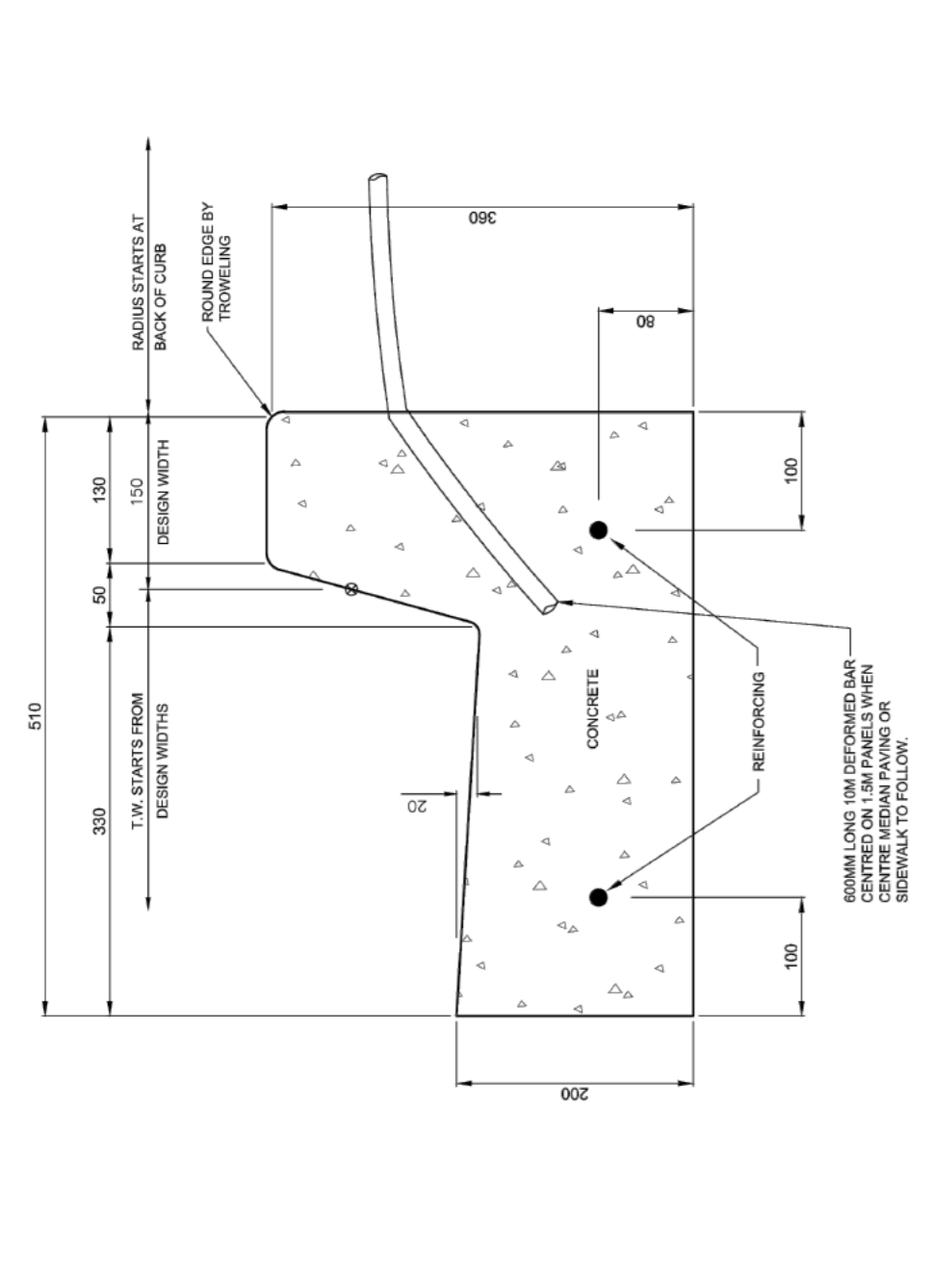
| Description |
|---|
| ISSUED FOR TENDER |
| NOTES REVISED FOR RIGHT IN - RIGHT OUT MEDIAN |

WSP REGINA
 Infinite Horizons
 City of Regina

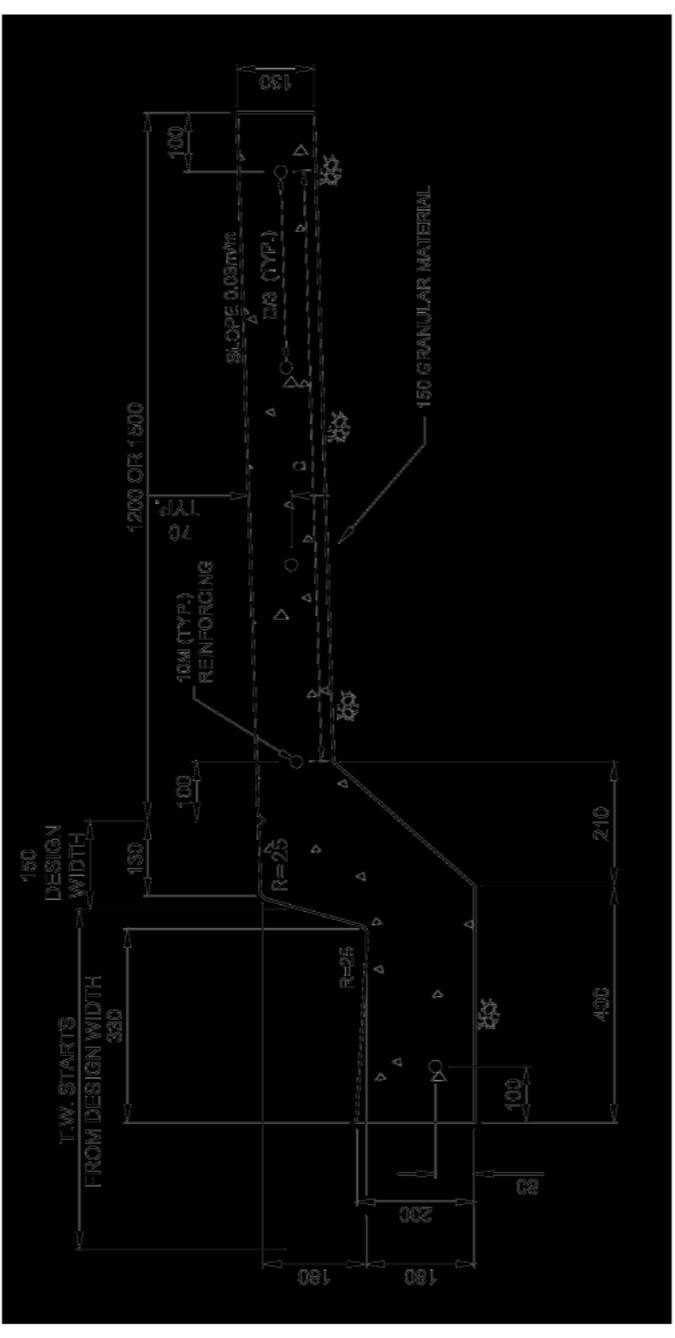
VICTORIA AVENUE

DETAIL PLAN - WEST

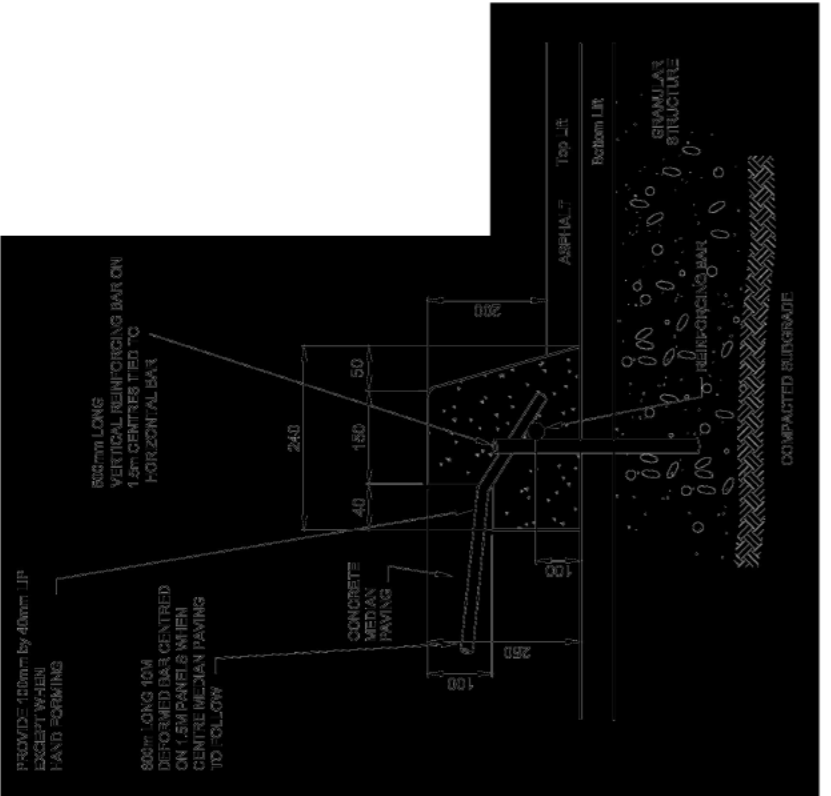
Date: MAY 13, 2015
 Design: Drawn: V.X.
 D.D. V.X.
 Contract No: 2435
 Scale: 1:500
 Project: VICTORIA AVENUE
 Digital File: 060700079
 Rev: C
 Sht: 12 of 21
 Cat: D-0005



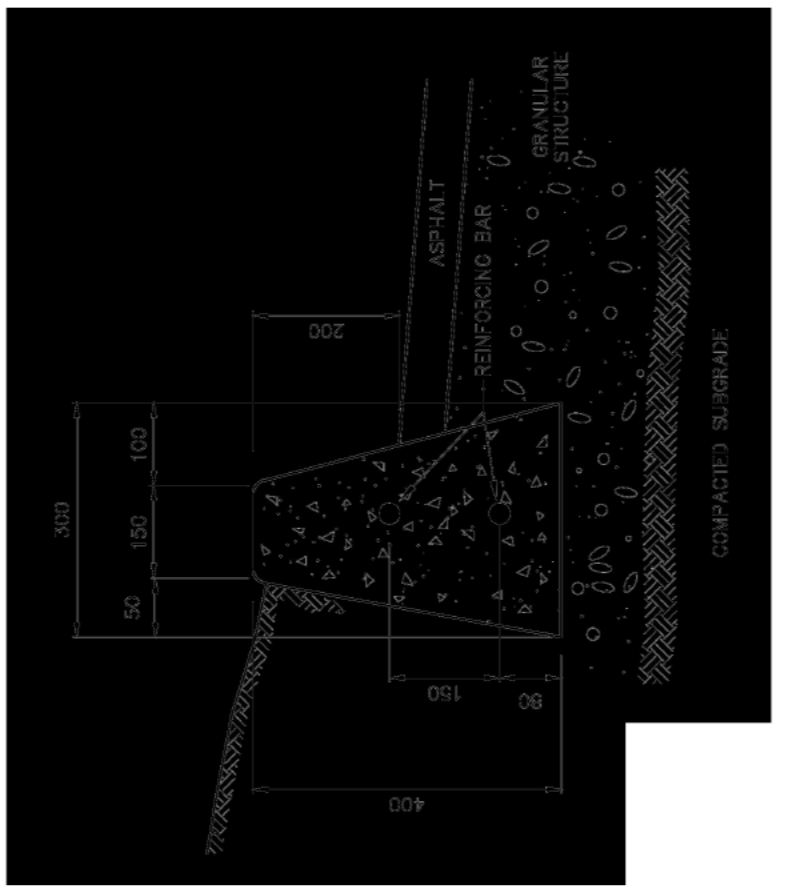
CURB AND GUTTER (R-4A)



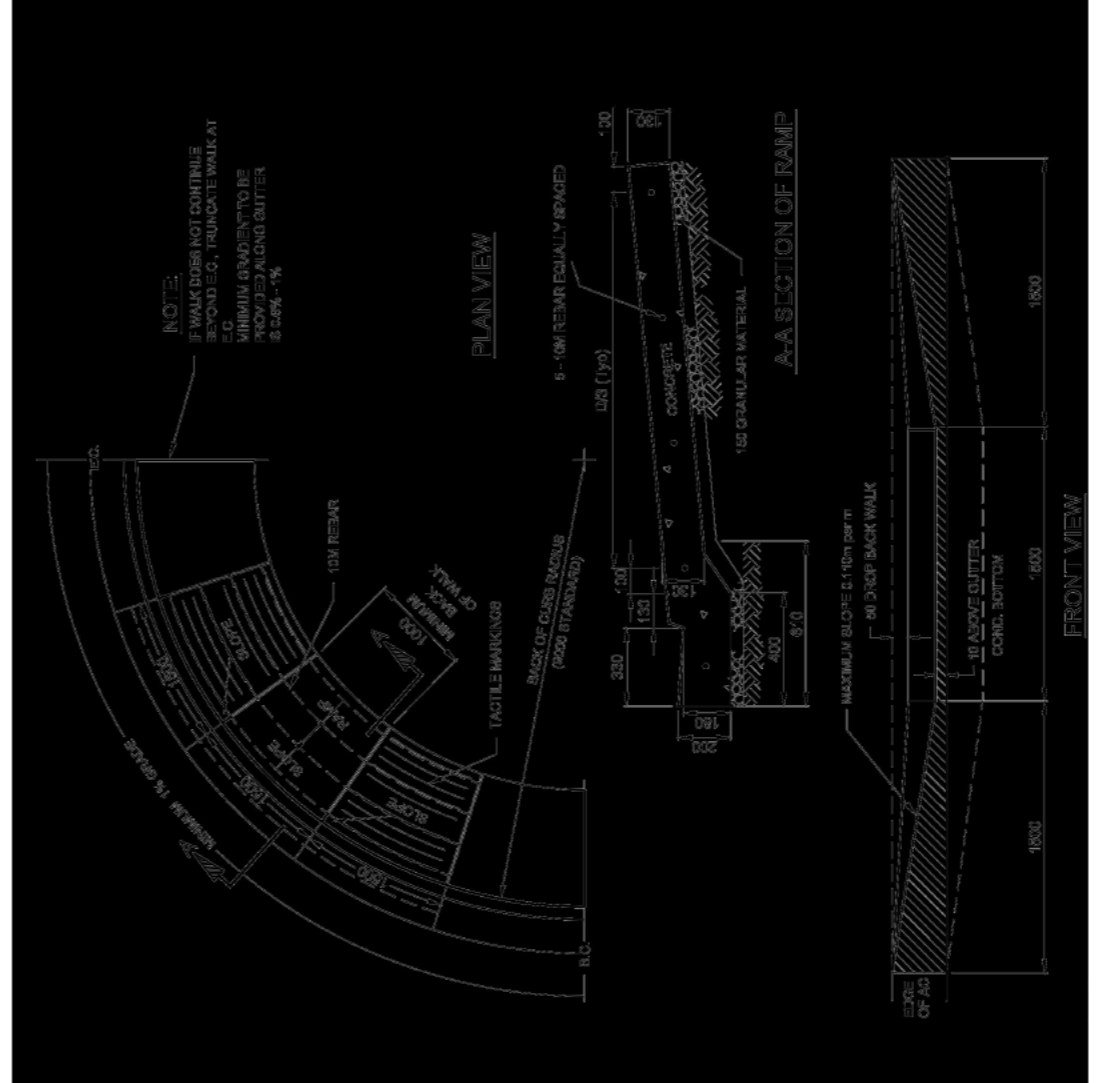
REINFORCED MONOLITHIC WALK, CURB AND GUTTER (R-7A)



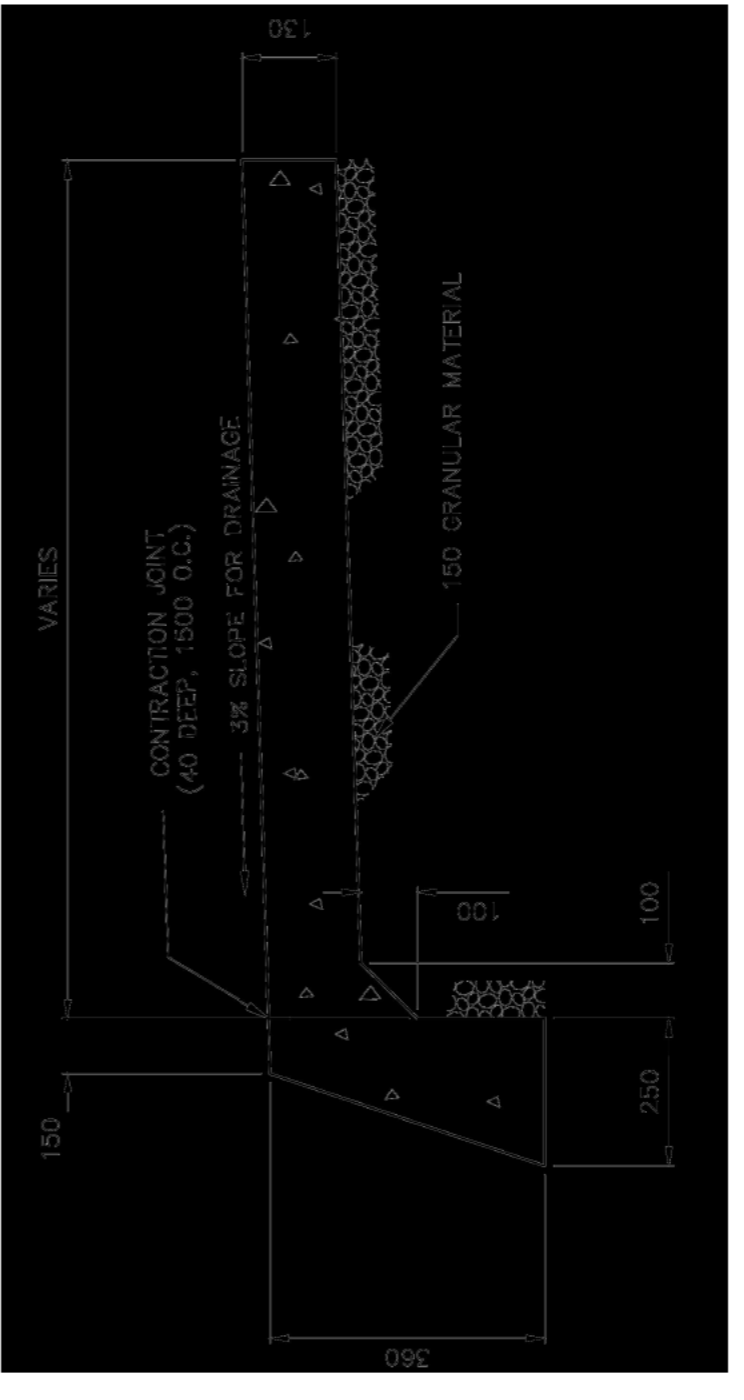
SLAB ON ISLAND CURB (R-5B)



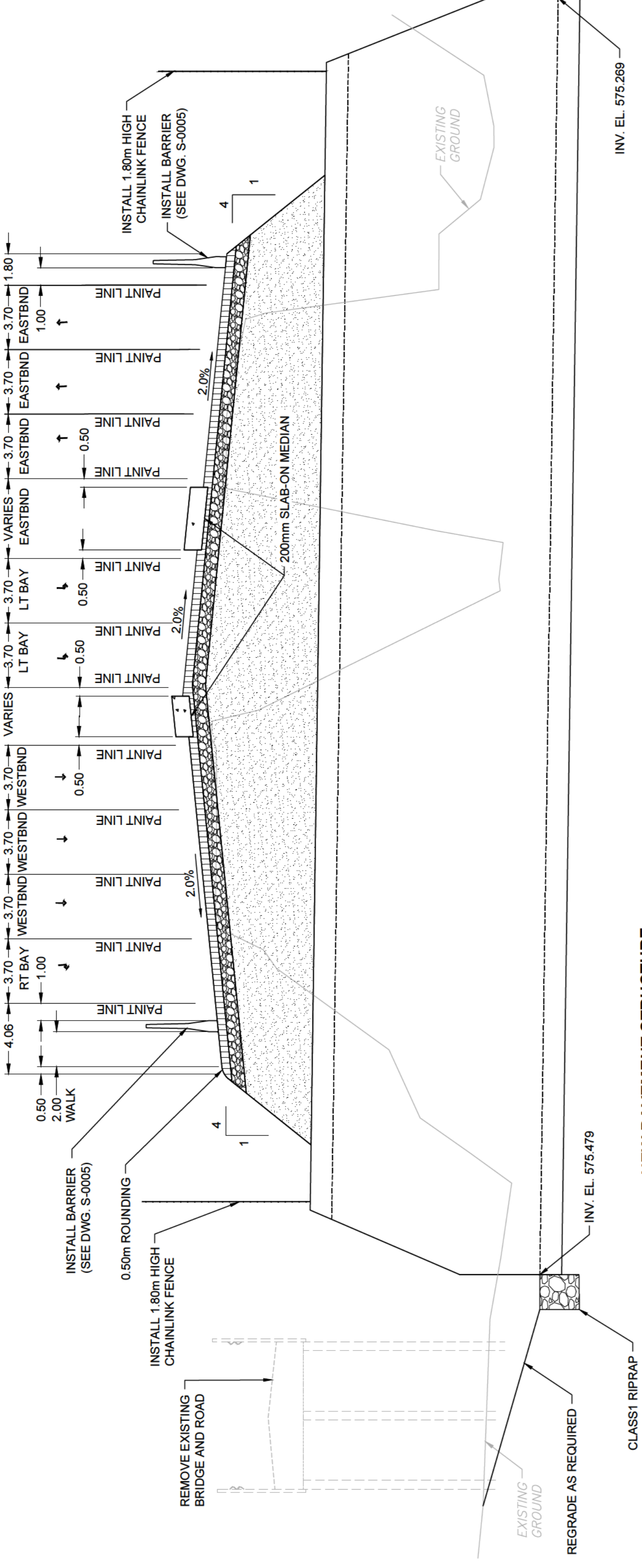
LANDSCAPED MEDIAN CURB (R-5)



PEDESTRIAN RAMP (R-9A)



COMBINED WALK AND CURB (R-7E)

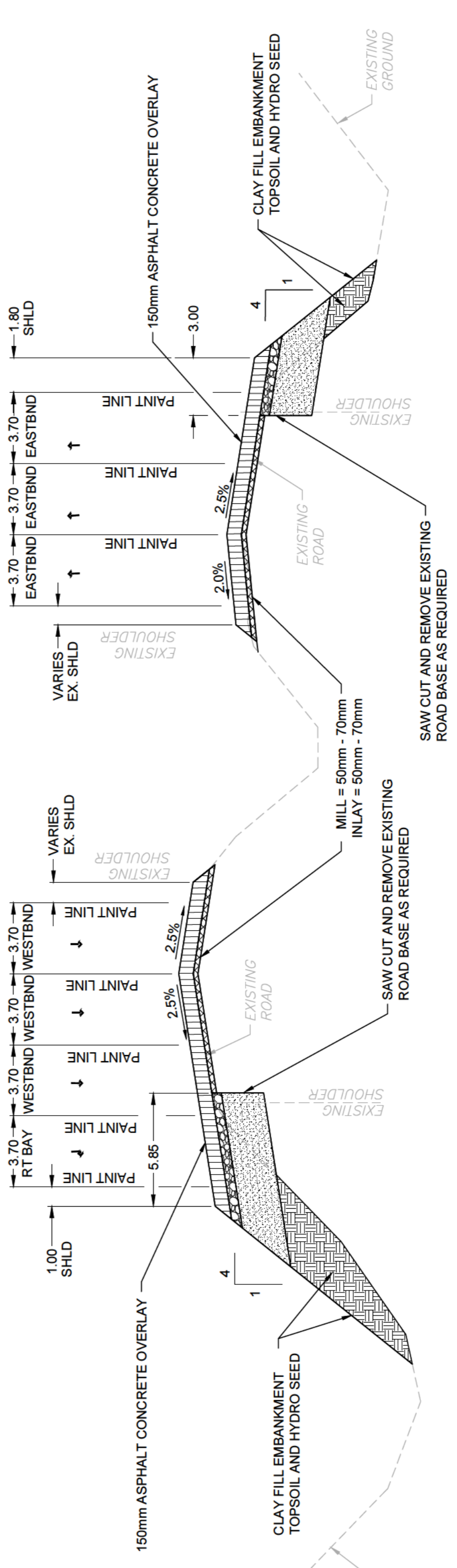


SECTION A-A

STA. 1+125
SCALE - HOR. 1:200 VER. 1:40

NEW PAVEMENT STRUCTURE

100mm ASPHALT CONCRETE
150mm TYPE 33 BASE
COMPACTED GRANULAR SUBBASE



SECTION B-B

STA. 1+290
SCALE - HOR. 1:200 VER. 1:40

NEW PAVEMENT STRUCTURE

150mm ASPHALT CONCRETE
100mm TYPE 33 BASE
440mm TYPE 8 SUBBASE
COMPACTED SUBGRADE

Notice:
-Utilities may not be as shown.
-City Crews must coordinate utility locations for ALL utilities (including traffic control facilities) through the City Dispatch Office.
-Contractors must coordinate utility locations through their own utility locators.
-The City of Regina is NOT responsible for any damage related to any inaccuracies in this drawing or any third-party contractor's failure to check drawings in the field.
-Drawings are in metric unless otherwise noted.
-Elevations are metric geoidic.

Professional Seal
Association of Professional Engineers & Geoscientists of Saskatchewan
CERTIFICATE OF AUTHORIZATION
WSP Canada Inc.
Number C0888
Permission to Consult held by:
Sk. Reg. No. _____
Signature: _____
Discipline: CIVIL
CIVIL: 22577

| M/D/Y | Ref | By | For |
|----------|-----|------|------|
| 04/28/15 | A | V.X. | B.M. |
| 05/13/15 | A | V.X. | B.M. |

ISSUED FOR TENDER
REVISED ADDITIONAL DETAILS FOR CONCRETE CROSS SECTION NOTES



VICTORIA AVENUE
STANDARD DETAILS & CROSS SECTION

Project: **VICTORIA AVENUE**
Date: **MAY 13 2015**
Design: **Drawn**
D.D.: **V.X.**
Contract No.: **060700079**
Scale: **1:500**
Cat: **D-0010**

Rev: **C**
Sht: **17**
21

Contract No. 2435

**Replacement of Victoria Ave East Bridges over Pilot Butte Creek
And Victoria Ave Widening**

ADDENDUM # 2

May 20, 2015

This Addendum is issued prior to Tender closing, for the above captioned tender call, to provide for revisions and/or clarifications detailed herein.

All revisions and clarifications become part of the tender documents and all of their effect is to be included in the tender prices.

The receipt of this Addendum must be acknowledged on the Form of Tender when submitted.

1. CLARIFICATIONS

- a) Is the contractor responsible for all project surveying including layout, elevations, second grades, as-builts, etc.?
 - i. The Engineer is responsible for establishing initial control points, QA of layout during construction as required, and complete as-builts following construction. The Contractor is responsible for surveying as laid out in Section 01050 - Field Engineering.
- b) If the existing streambed has to be deepened due to unsuitable soils is granular base to be used to raise founding grade back to founding elevation? Do the existing concrete piles have to be cut off deeper?
 - i. This would be dealt with at time of construction if need arises, but we would use the same granular material as underneath the culverts as shown on the plans. Existing concrete piles are to be removed below grade.
- c) Clause 7.16.2 of the special provisions states that the Clay Seal Backfill cannot consist of highly plastic clay or high silt content clay. Given that these types of materials make up virtually all of the natural soils in and around Regina; is it expected that imported Clay till would have to be accessed and hauled significant distance for usage as the approved product? What are the specified plasticity requirements for the material?
 - i. High Plastic clay is acceptable. The intent is to have an impermeable layer. If High Plastic clay is used, compaction must be to 100% of proctor, with a moisture content within 2% (+/-) of optimum.
 - ii. High Silt content clay is not acceptable.
- d) Clause 7.8.1 of the special provisions states "all materials shall be subject to inspection, sampling and quality assurance testing by a third party retained and paid for by the

Contractor “. The Standard Construction Specification – Section 1450 states that the Contractor is responsible for the Quality Control testing and the owner is responsible for Quality assurance testing. There appears to be a conflict in the statements which requires some clarification. Which is correct?

- i. For QA testing of the box culverts, product from an OCPA Prequalified Plant will be acceptable in lieu of independent QA testing. The QC testing as per Section 1450 will still apply.
- e) In regards to Drawing number S-0007, Can the dimensions of the culvert be in imperial dimensions? I.e. Instead of 250mm thick wall it would be 254mm (10”) the width would be 3556mm (140”) etc.?
 - i. As long as the hydraulic (interior) dimensions of the culvert meet or exceed the dimensions shown on the plan, the dimensions of the box culverts can be in imperial if necessary.
- f) There appears to be a discrepancy on two different drawings (legend on drawing D-0005 and section A – A on drawing D-0010) related to the Asphalt pavement structure thicknesses. What is the correct asphalt, Granular Base, Sub Base and Sub -Drainage sand thicknesses over top of the new bridge culvert section?
 - i. Rev. C of Drawing D-0010 shows the correct pavement structure on the new culvert.
- g) What are the Asphalt, Granular Base, Sub base and Sub Drainage sand thicknesses to make up the pavement structure on the detour sections?
 - i. The required structure for the detour is 100mm Asphalt Concrete, 200mm Type 33 Base, and 300mm Type 8 sub base over any required fill for grade.
- h) Is it anticipated that the temporary fill for the stage 1 detour immediately adjacent to the new box culverts will remain in place as fill behind the new north retaining walls?
 - i. The Lock Block retaining wall is required to retain all fills during staging and final alignment.
- i) Is there a designated unloading/ staging area for the box culverts?
 - i. A portion of the park space to the north will be accessible to the Contractor as an unloading area – the Contractor will be responsible for maintaining and reestablishing after completion.
- j) There is mention of taking photographs of Sealer application in Section 7.4, Clause 1.7.1 in the 00700 - Special Provisions. What process and portion of the project is this referring to?
 - i. There is no Sealer application required on this project; disregard point referring to Sealer application.
- k) In the case of a late award, there may be delays in production/curing /delivery of pre-cast box culverts and/or precast barriers. These delays may be longer than the actual award delay. If this occurs, how will this additional delay be dealt with?

- i. It is the Contractors responsibility to schedule the coordination of the work and bid the required number of site rental days required. There are other portions of the projects that could begin if delivery of the precast items is delayed.

- l) On some projects in the past, there have been some issues with the City Landfill not accepting the black organic soils from the creek bottoms even though there was no contamination. Is there any anticipated issue with the creek bottom material from this project?
 - i. There is no contaminated soil in creek bottom to the City's knowledge, and there should be no issues with the City Landfill.

- m) The GAW mesh indicated in the chain link fence specifications is not available anymore. Only GBW (galvanized before weaving) is made these days. Also, two types of fence are shown Type A - no top rail and Type B - with top rail. Which type do they want? Are there any gates on this fence?
 - i. GBW is the correct material. The fencing should be Type B – with top rail.
 - ii. There are no gates required for this project.

END OF ADDENDUM NO.2

Contract No. 2435

**Replacement of Victoria Ave East Bridges over Pilot Butte Creek
and Victoria Ave Widening**

ADDENDUM # 3

May 22, 2015

This Addendum is issued prior to Tender closing, for the above captioned tender call, to provide for revisions and/or clarifications detailed herein.

All revisions and clarifications become part of the tender documents and all of their effect is to be included in the tender prices.

The receipt of this Addendum must be acknowledged on the Form of Tender when submitted.

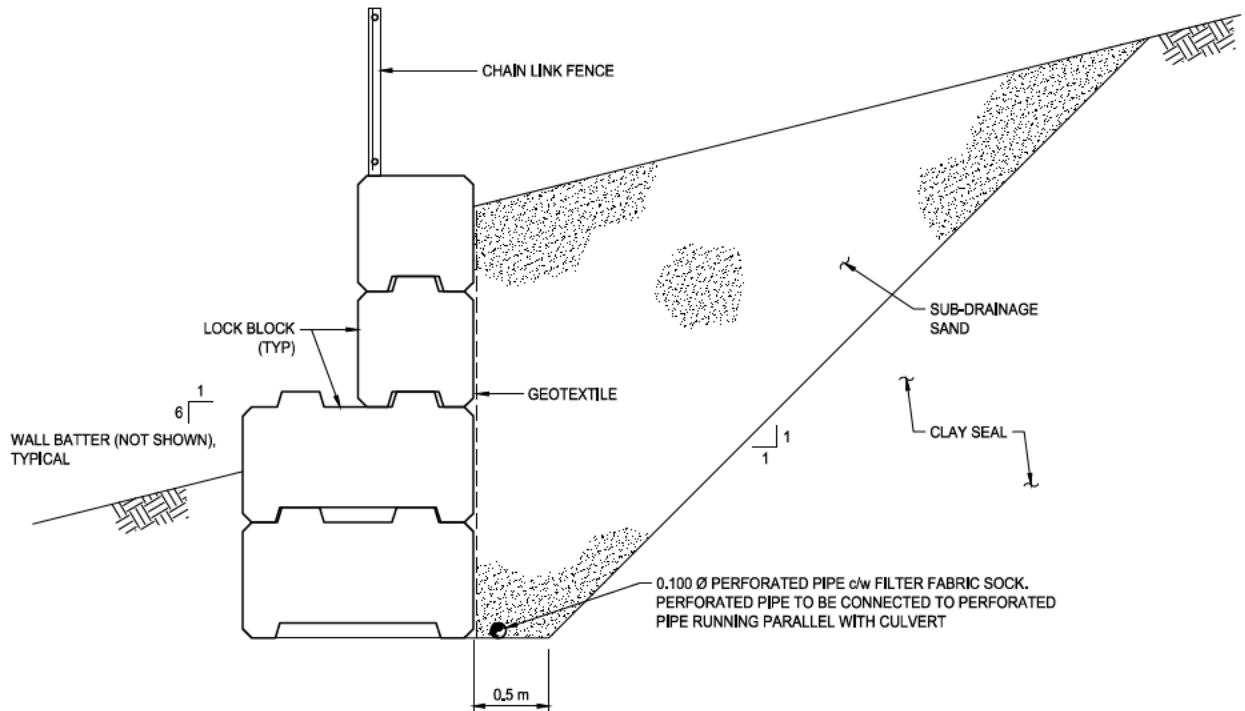
1. CLARIFICATIONS

- a) Are the lengths of the box sections on drawing S-0007 critical? Will the owner have any issues with longer or shorter section that total the same length and address the intermediate construction positions required at specific times?
 - i. Provided the sections can be shipped, handled, and installed safely we have no issues with different lengths than what is shown. The City will accept alternate lengths, though the standard size utilized should be no shorter than 1.5m in length (excluding the 1.0m sections required as shown on the plans).
- b) Is there any requirement for galvanizing or epoxy coating on any of the reinforcement in the box sections?
 - i. Note 7 on sheet S-0007 indicates the design life of the box culverts is to be 75 years. This should be taken into account when designing the reinforcement and cover for the boxes and it will be up to the fabricators engineer to determine if galvanizing or epoxy coating is needed to achieve the required design life.
- c) Is the slab on median a full 200mm thick right through? Or can we slip the curb which we have a 250mm mold with shelf and then base it up and pour 100-130mm for infill?
 - i. The slab-on median does not have to be 200mm throughout. The slab-on medians can be constructed as per City of Regina Standard Drawing R-5B, Slab on Island Curb as shown on D-0010 Rev. C
- d) The mono areas and curb and gutter are unclear. The tender quantity for curb and gutter seems to include the curb and gutter from the mono. Please clarify these items.
 - i. The tender quantity for curb and gutter *does* include the length of monowalk as well as the straight curb and gutter. Though it is expected that the Contractor pour the curb and gutter monolithically with the walk, the items are measured and

paid for separately. Bid item 18 (Combined Concrete 1.5m Walk and Curb) and Bid Item 21 (Concrete Median Curb) are independent of the above, as there is no gutter portion.

- e) The attached sketch, SK-01, shows the extent of the sub-drainage sand wedge behind the retaining walls, complete with weeping tile. These are the approximate limits of the wall relative to the clay seal behind.

END OF ADDENDUM NO.3



NOTE:
CONTRACTOR TO ENSURE COMPACTION BEHIND BLOCKS DOES NOT
RESULT IN MOVEMENTS OF BLOCKS DURING INSTALLATION

TYPICAL RETAINING WALL SECTION

1:50

Contract No. 2435

**Replacement of Victoria Ave East Bridges over Pilot Butte Creek
and Victoria Ave Widening**

ADDENDUM # 4

May 25, 2015

This Addendum is issued prior to Tender closing, for the above captioned tender call, to provide for revisions and/or clarifications detailed herein.

All revisions and clarifications become part of the tender documents and all of their effect is to be included in the tender prices.

The receipt of this Addendum must be acknowledged on the Form of Tender when submitted.

1. CLARIFICATIONS

- a) How were the coordinates shown in the plans derived? Where is the base location? Where are the control points? What Datum are the coordinates in?
 - i. This information will be provided to the successful bidder upon award.
- b) Is the bridge coordinate system and the road coordinate system the same?
 - i. Yes
- c) The stationing for the project begins and ends at the limits of construction. How am I able to determine the positions of the limits based on the information currently provided? This is extremely important as it allows us to setup the job correctly in the first place.
 - i. This information will be provided to the successful bidder upon award.
- d) How will proper drainage be ensured on the new pavement surface? I am assuming cut/fills will be required on the existing and milled pavement surfaces? With that being said there are no elevations shown in the drawings regarding the pavement surface.
 - i. The roadway is crowned, as per the cross section shown. As noted on the cover page of the drawings, the grading plans were not included with the tender set but will be part of the drawings issued for construction.
- e) Drawings D-0001 to D-0007 do not have any elevations specified on the plans; only the location. Why is this?
 - i. As noted on the cover page of the drawings, the grading plans were not included with the tender set but will be part of the drawings issued for construction.
- f) The turn lane onto East Gate Drive and East Gate Drive itself is lacking information – Drawings D-0005 to D-0006, I take we are to receive more information on this soon?
 - i. Yes. Any additional information required will be provided to the successful bidder upon award.

END OF ADDENDUM NO.4